

# Village Street Corridor Design Handbook

## Main Street, Park Street and Gilbert Street

Village of Cambridge,  
Washington County,  
New York



Adopted By  
The Village Board  
Of Trustees  
September 21, 2005

Prepared by:  
J. Kenneth Fraser and Associates, PE, LS, LA, P.C. and Cynthia Behan, RLA

**Main Street, Park Street and Gilbert Street Visualization Project and Design Handbook for the Village of Cambridge,  
Washington County, New York – Adopted By The Village Board Of Trustees September 21, 2005**

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### Introduction

The historic Village of Cambridge, nestled in the scenic Cambridge Valley of rural Washington County has much to be proud of. A tradition of preserving local history, a tradition of cultural, religious and artistic expression, and a history of agricultural commerce have helped shape the village. The Cambridge village landscape is to be admired. It includes a thriving commercial Main Street, a consistent and unique grid of streets, and an excellent mix of housing types and convenient services that meets most local needs. However, as change is inevitable, it is recognized that a clearly articulated expression of intent, is necessary to appropriately support the desired vision of the future.

This project is a direct outcome of the 2004 Comprehensive Plan prepared by the Saratoga Associates. Many of the important issues identified by the local citizenry during that planning process provided a basis for discussions leading to the development of this handbook. Specifically, the 2004 Comprehensive Plan identified the following goals to guide decision-making:

- *Expand opportunities for activity, recreation, education, gathering, and interaction between and among all groups of village residents. In particular, expand opportunities for youth and for seniors in the community.*
- *Encourage continued growth that is consistent with the other goals for the Village. Promote development that enhances economic opportunity and*

*community well being while considering the resources upon which our economy and our community depend in the long-term (“sustainable development”). Through sustainable development we aim to meet the needs of the present without compromising the ability of our community to meet its needs in the future.*

- *Maintain the character and enhance the vitality of the three existing commercial / mixed-use nodes along Main Street (around the intersections with Union Street, the railroad tracks, and Park Street). Ensure that these areas remain the focal point for business, social, cultural, and civic activity in the Village and the region. Address parking, transportation, pedestrian mobility, infrastructure, business development, historic resource preservation and community character concerns to allow these areas to thrive as the heart of the community.*
- *Enhance the sense of arrival to Cambridge by improving the “gateways” to the Village. Support efforts to discourage sprawl and enhance agriculture in the region.*

The consultant team of J. Kenneth Fraser and Associates and Cynthia Behan, had as its’ goal to derive design concepts and ideas from local participants in open public workshops. The team explored the village’s primary street corridors and assisted workshop participants in developing methods of supporting the above Comprehensive Plan goals.

Several fascinating ideas and designs materialized during the workshops, and as a result helped form a basis for the organization of the handbook that would guide future development. Clear priority was placed on major intersections such as Main and Park by workshop participants, thus focusing the consultant team’s efforts on specific areas that were important to Cambridge.

This document begins with a brief analysis of factors outside of the subject main road corridors. Those factors included local history and culture; open space and recreation; economic and community development; and pedestrian comfort and safety. The consultant team was able to gain a better understanding of how outside factors might affect the corridors, thus resulting in more informed conceptual designs.

**The conceptual designs presented in this handbook should not be considered final designs or decisions by the village. Rather, they should be utilized as templates from which to further develop ideas via additional focused public participation. The designs do communicate a local vision for the future and will provide a needed communication tool for that vision.** The existence of this document demonstrates a motivated, organized and participatory approach to planning for the future. The guide, by example, will assist applicants in the development of site plans and it will support the review of those plans. This approach prepares Cambridge for future changes by presenting a positive model of what is desired.

Further, the document represents a significant advantage to the village when the need for outside funding for projects arises. Granting agencies will

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want to see how a specific project fits into an overall community vision. The document also includes suggested sources for that funding.

Generic details and standards are presented to help guide future design decisions. Recommendations are presented for specific kinds of street furnishings. These furnishings were either favored by the village, or represent a compatible historical style.

+ Several areas could not be detailed within the scope of this project. As a result a number of off-shoot studies are recommended, including:

- Certified Local Government
- Uniform Signage System
- Business Improvement District Plan
- Business Demand Analysis
- Traffic and Access Study
- Historic and Interpretive Plan
- Open Space and Recreation Plan
- Bicycle and Snowmobile Trail Plan

Finally, the plan presents a few code changes that might be developed to encourage sensitive private developments, including innovations such as form-based code, shared-use parking code, and a mixed use building code.

A suggested phasing plan, presented together with a list of funding sources completes the handbook.

This document provides graphic representation for design ideas that can help Cambridge realize its' vision. As such, it represents a starting point, not a completed plan.

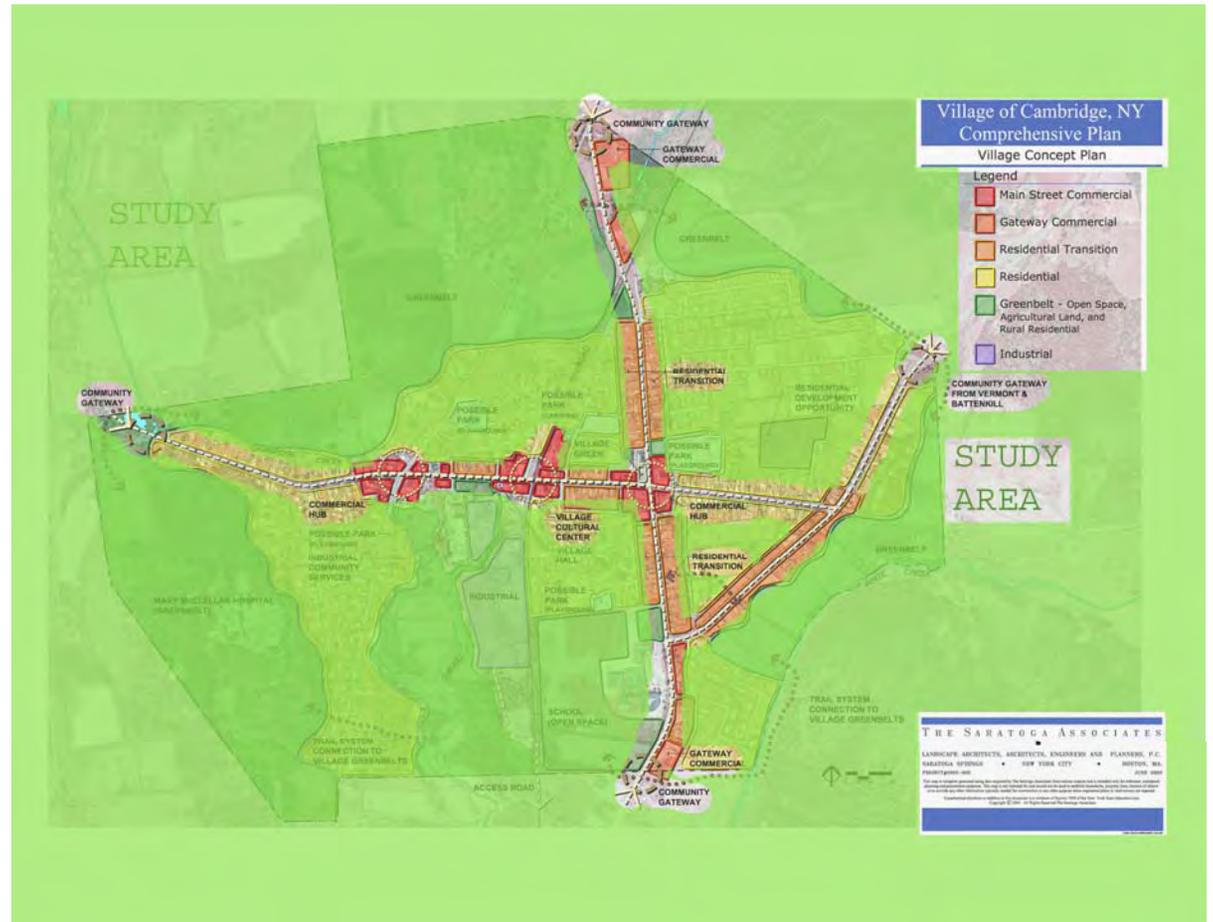


Figure 1: Study Area Overlay on the 2004 Concept Plan prepared by the Saratoga Associates.



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### 1. Local History and Culture:

The celebration of the rich and varied history of Cambridge provides wonderful opportunities to enhance the character of the Street corridors.

Elements including signage, banners and pavements could provide historical interpretive information. New architectural elements and focal points could be designed around reproductions or adaptations of significant historic elements.

During workshop discussions about history, a number of significant places, people and events were recounted. These were organized into several themes which would provide opportunities to develop interpretive trails and tours. The suggested themes include:

- Early history of Cambridge (pre 1761)
- Revolutionary War (training grounds, trails, folklore, cemeteries)
- Railroads in Cambridge
- Village Agricultural History – (Lovejoy Plow, Rice Seed, Flax, Potatoes)
- Village Heritage – (local industry, fairgrounds, significant architecture, churches, uniquely

- Cambridge features, local artists)

Other history and heritage related ideas for the Main Street corridor include creating an interpretive center or “trail-head” for trails, and tours near the freight yard and railroad crossing. Several historical uniquely Cambridge street features provide archetypes for future street projects, including yellow brick pavements, and rehabilitation of “pork chops” for historic interpretive elements. Recalling lost architectural elements should also be part of the plan including adaptation of lost elements such as fairground gazebos, a curved coal yard trestle, yellow brick roads, etc. The Village should continue to aggressively seek preservation planning and capital grant funds to advance their preservation goals and enhance the experience of the Village for tourists.

Additional work will be needed to fully develop an interpretive system that includes kiosks and displays, trails, brochures, and other elements. Throughout this handbook, attempts to link to Village history could be interpreted in pavements, materials, architectural forms and placement of new features. Most importantly, historic materials and forms should be used, or at a minimum recreated wherever possible.

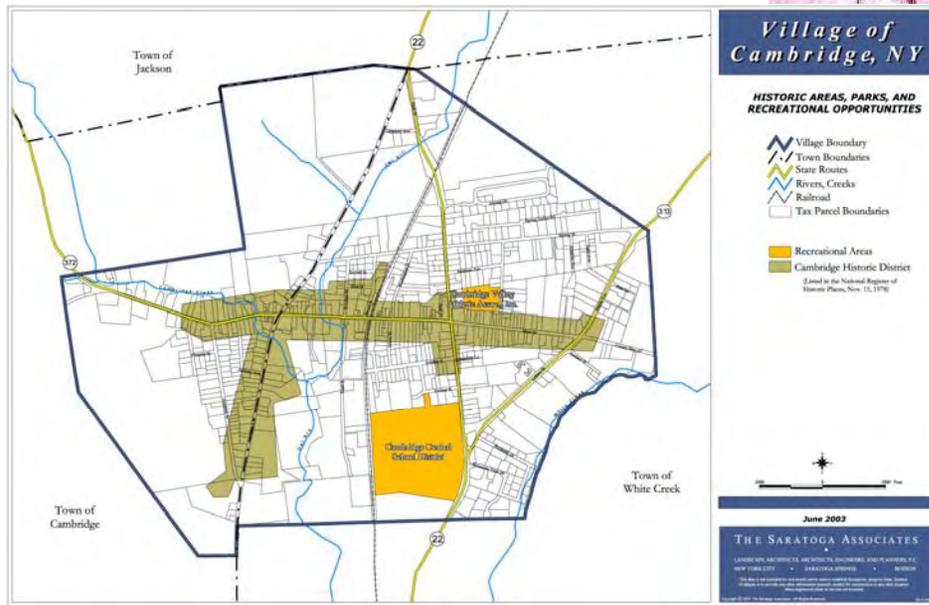


*Photograph 1: Existing Entrance Signage.*

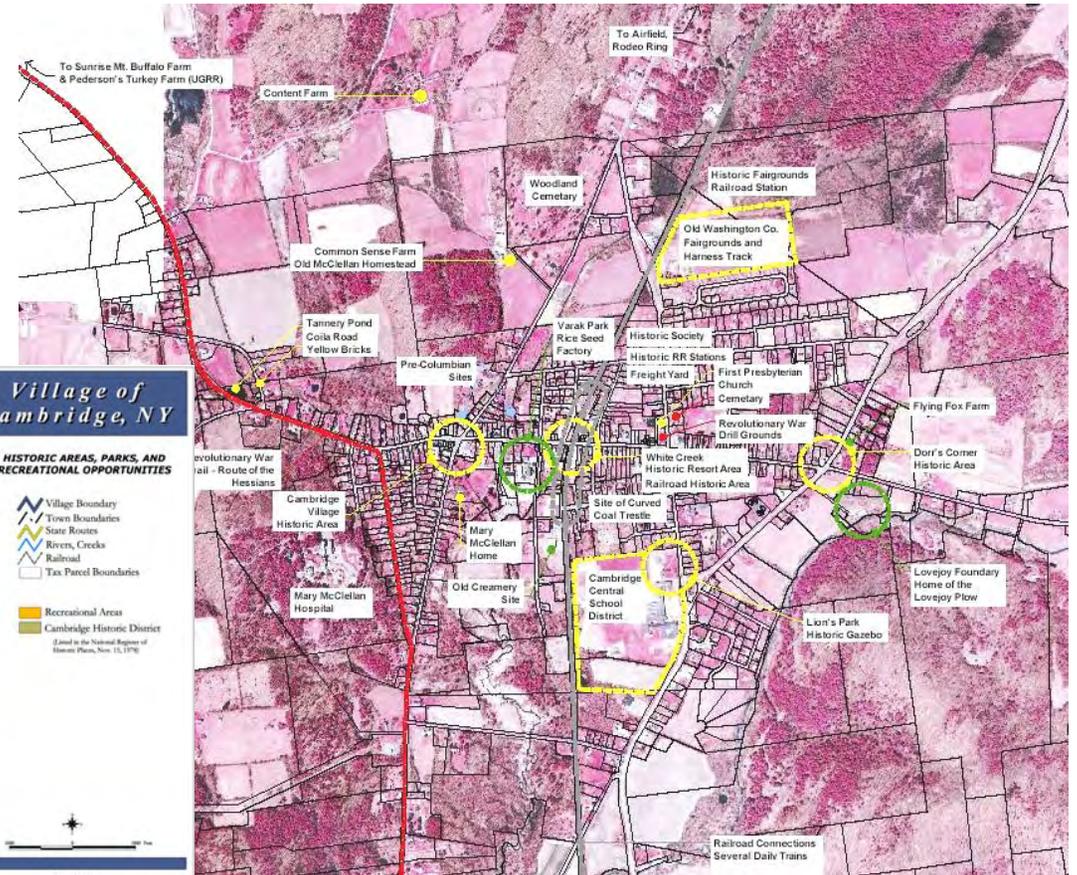
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## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

The Historical and Cultural Features Map illustrates some of the significant historical and cultural features identified during the workshops. The map also identifies these elements utilizing a color coding system on signs, pavements, and structures that may help suggest a simple way to identify different themes, trails and tours. Where linear elements, known trail routes or links exist, they are shown on the map.



Map 1: Historical Areas, Parks, Recreational Opportunities (2004 Comprehensive Plan)



Map 2: Historical and Cultural Features

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### 2. Road Corridor

#### Safety and Comfort:

Improved safety and comfort along the road corridors were key factors in the 2004 Comprehensive Plan that led to the development of this Design Handbook. At present, the pedestrian/bicyclist system does not provide for a sufficient level of safety or comfort within the business district or at the busiest intersections. Sidewalks are lacking where needed and pedestrians/bicyclists are forced to cross great expanses of road or parking pavement to reach destinations. Uncontrolled vehicle access along commercial road frontage discourages walking/bicycling and creates dangerous pedestrian/bicyclist and vehicular conflicts. As a result, there is an avoidance of walking/bicycling altogether by the young and the elderly. Residents feel that there is insufficient signalization or signage for pedestrians/bicyclists. Some public pedestrian/bicyclist amenities are placed along routes, but residents do not feel they are sufficient. Lighting of pedestrian areas is also a concern as sufficient light at a pedestrian scale is lacking throughout the Village. Key elements of a reconstituted pedestrian/bicyclist-friendly road corridor should include:

- Create better continuity and connectivity along pedestrian/bicyclist corridors/links and destinations.
- Evaluate existing pedestrian/bicyclist corridors/links and identify more appropriate alternative corridors/links for improvements.

- Introduce traffic calming elements to slow traffic, creating a more compatible mix for pedestrians/bicyclists and vehicles.
- Improve crossing areas with visual cues such as: textured/colored pavement treatments, lighting, signage, and curb extensions that reduce crossing length and increase visibility.
- Improve comfort and shelter by providing appropriate street trees, bike racks, information signage, kiosk/shelters, and seating areas.
- Provide adequate and appropriate pedestrian/bicyclist scale lighting in residential areas, commercial areas, with particular attention to improving pedestrian/bicyclist corridors/links and destinations.
- Provide pedestrian/bicyclist oriented signage to identify improved corridors/links and destinations.
- Respond to specific needs for alternative (separate paths, or along parallel streets) pedestrian/bicyclist systems, especially to connect residential areas with youth areas and schools.



*Photograph 2: Uncontrolled Access in front of Rite Aid at Main and Park.*



*Photograph 3: A 70 foot wide roadway at Main and Union is difficult to safely cross.*

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### 3. Economic and Community Development:

Enhancements to the transportation corridors can be extremely beneficial to local businesses. During the Economic and Community Development group discussions at the Workshops, business owners and customers were most concerned with the availability of ample parking and the visual appearance or “curb appeal” of commercial areas. A more consistent and “pedestrian friendly” appearance would make the Village’s commercial areas more readily identifiable, inviting, vibrant, active... successful. Well identified and ample parking is an obvious need. Secondly, the corridors/links need to address “user comfort” by creating an easily identifiable, inviting system that will facilitate and encourage use and increase activity.

“Triangulation” is a term for the activities, uses, or economics that occur indirectly from other activities. Triangulation results from a “critical mass” and a clearly articulated pedestrian system that supports 2<sup>nd</sup> and 3<sup>rd</sup> order activity that otherwise would not alone generate the activity.

To increase the identification and use of available parking, several off-street and on-street ideas were developed during the workshops.

#### On-street Solutions:

- Diagonal parking on West Main & South Union
- Striping of parallel stalls in commercial areas

#### Off-Street Solutions

- Develop share-use lots on private properties through cooperation between owners
- Provide better connectivity between off-street lots and common parking areas
- Develop common use plazas that can function as parking for special events - overflow situations, that are primarily open space or plaza space

#### Visual/Comfort Enhancements:

- Better plantings – more appropriate trees/shrubs
- More benches, trash receptacles and bicycle racks
- Better, more appropriate lighting
- More attractive and varied pedestrian pavements
- An improved signage system

#### Other Improvements:

- Establish a Business Improvement District (B.I.D.)
- Involve local businesses in creation of a “uniquely Cambridge “ look
- Assess local service, retail and other commercial needs in a Market Analysis



*Photograph 4: Streetscape improvements that create an active commercial district are an economic and community development goal.*

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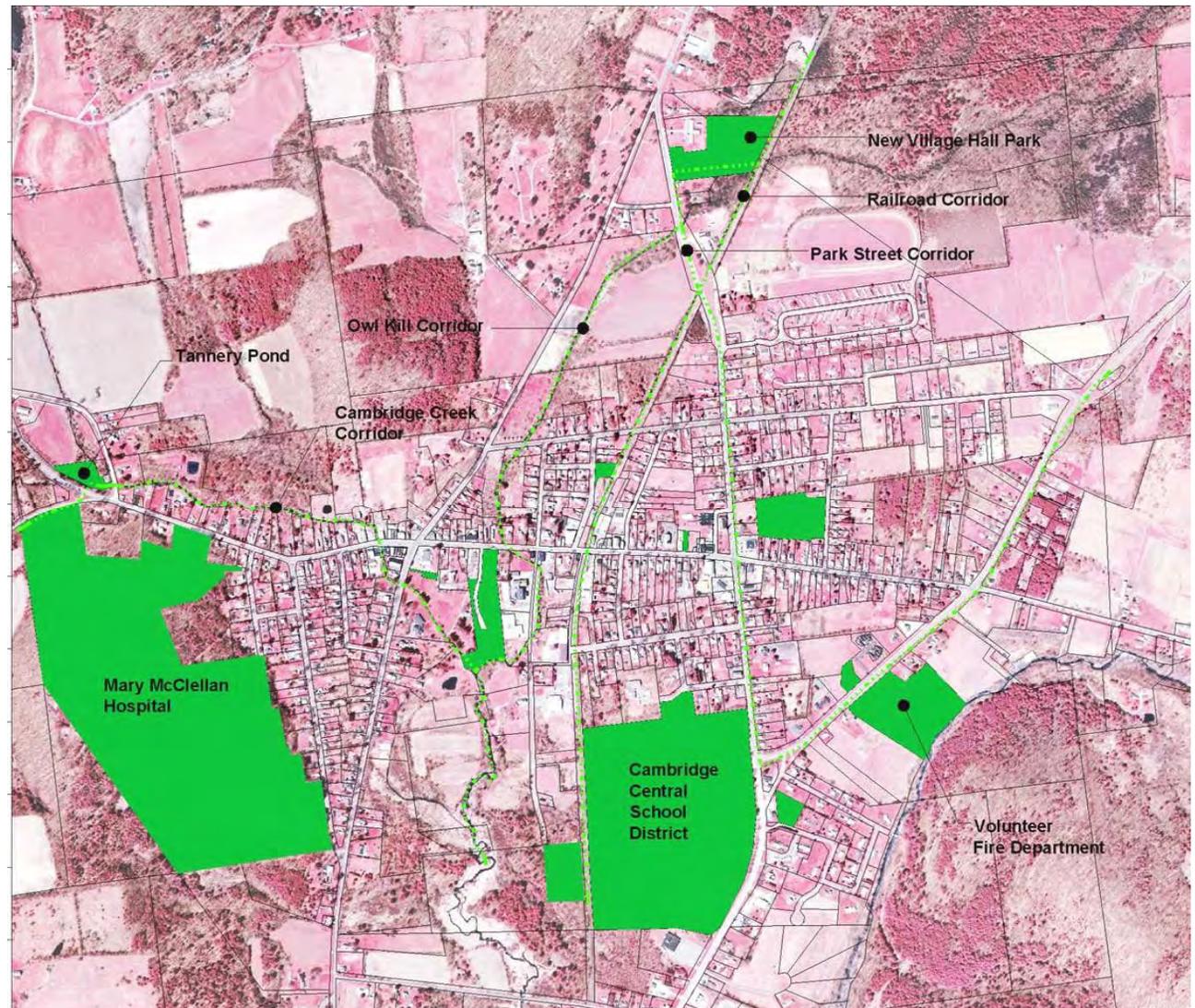
## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### 4. Parks, Recreation and Open Space

Few public parks exist in the Village; however there is considerable public and private open space that could be configured into an open space system. During the development of the Comprehensive Plan, this lack of park space was identified as was an associated lack of youth-oriented recreation opportunities. In response, the Village is seeking funding to develop a playground, skateboard park and nature trail on public lands along North Park Street.

Other recreational-related needs expressed during the workshops included an indoor recreation center with a pool. Outdoor performance space in a dedicated location for the summer concert series is needed, preferably close to seniors and the school.

The Open Space Opportunities Map (Map 2 – This Page) illustrates several lands that could be made public through use agreements or easements, lands that are already public but not widely used, and vacant lands not suitable for development identified in the 2004 Comprehensive Plan. These “opportunities” provide guides that can be linked to or expand other initiatives such as history and heritage preservation, pedestrian safety, and off-street parking creation.



Map3: The map above illustrates open space and trail corridor opportunities. The dashed green trail corridors provide excellent connectivity between existing and future open space recreation resources.

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## C. Concept Plans and Illustrations

The plans and illustrations that follow are based on conceptual ideas regarding critical “focus areas” during the workshops. Illustrations in this section are presented in plan renderings, para-line renderings, perspective sketches, and before and after photo-simulations. The concept plans from the workshops formed the basis from which detailed ideas were refined and basic graphic standards applied. Later sections will present standard details to be used outside of these focus areas, and typical street furnishings to be considered for all of the projects.

The conceptual plans presented in this Design Handbook should be considered guides for development and not concrete plans. Many of the ideas presented can be implemented with small investments of time and money, while others will require more intensive financial commitments and additional financial assistance from government grants.

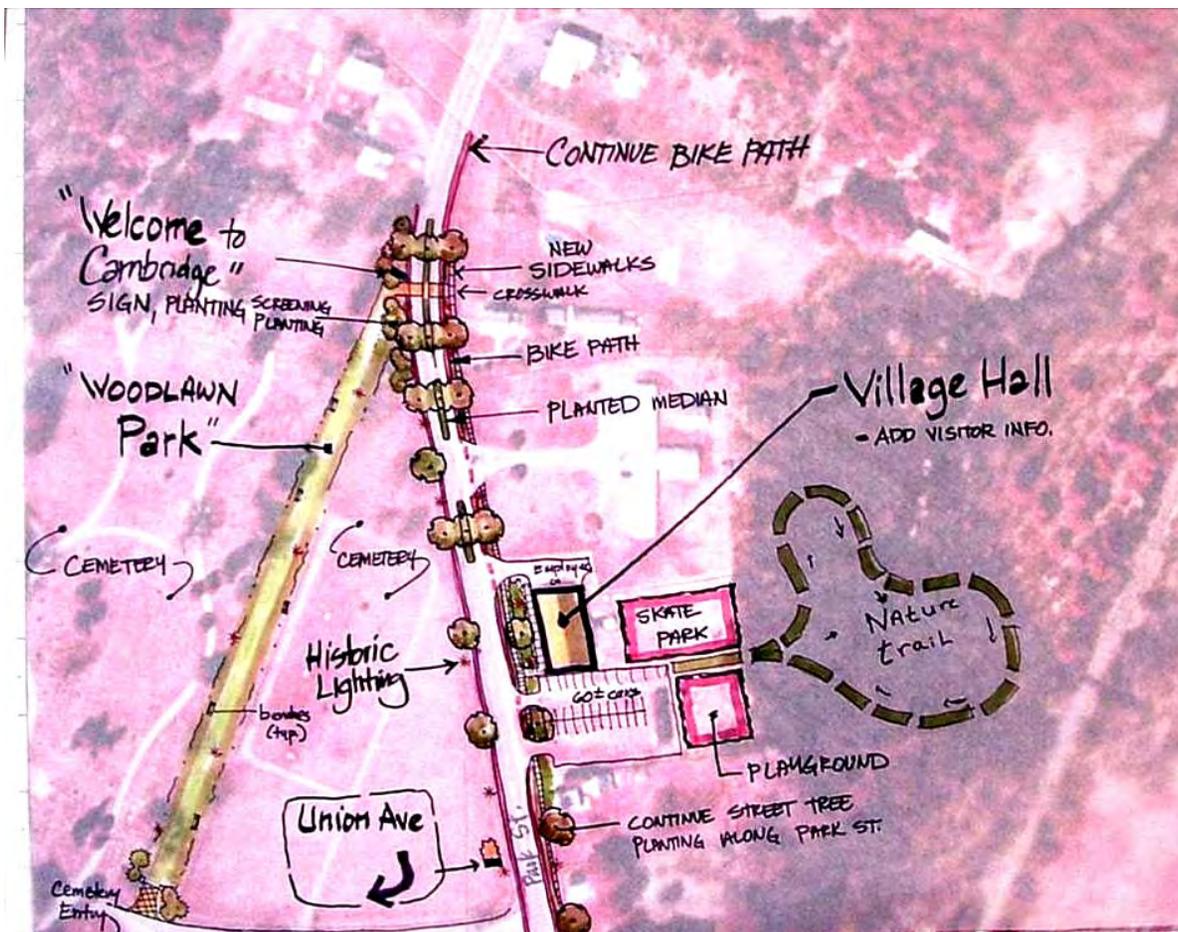


Figure 2: A conceptual plan developed during the visioning workshops.

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### 1. Main Street at Park Street Intersection

The most traveled intersection in the Village is also the least friendly towards pedestrians. The Main and Park intersection is characterized by overhead wires, parking, and commercial access. Due to the angle of the intersection, Cumberland Farms and Rite Aid are the most prominent features while approaching the intersection in either direction on Main Street. The beauty of the steeples First Presbyterian Church is diminished by the profusion of overhead wires and poles. On Main Street sidewalks end a couple hundred feet short of the intersection at Rite Aid and Cumberland Farms. Uncontrolled access to both streets from Cumberland Farms and Rite Aid creates a dangerous condition few pedestrians are willing to risk crossing. In spite of all the shortcomings, there is great potential for low cost and effective improvement at this intersection.

#### STRATEGIES:

- Improve Pedestrian Facilities and Continuity Throughout
- Create Greenspace, Public Spaces and Install Additional Trees
- Create a Focal Point at Rite Aid
- Bury or Relocate Overhead Utility Lines

- Coordinate and Time Truck Access for Better Safety
- Re-work Intersection for Pedestrians while Providing for Truck Turning
- Organize Parking and Access to Expand Parking/Create Open Space
- Look for Shared Parking Opportunities
- Provide Benches and Shelters for Pedestrians
- Consolidate Signage on Common Sign Standard



Figure 4: The Popcorn Wagon



Figure 5: A sketch of the suggested focal point for the Rite Aide corner includes a structure intended to recall the curved coal trestle that once existed in the rail yard south of Main Street. The structure in the foreground is intended to house the 'popcorn wagon'.



Figure 3: Cumberland Farms corner improvements might include a small fountain.

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**Main Street, Park Street and Gilbert Street/Maple Avenue Corridors**



Figure 6: Main Street and Park Street Intersection Sketch

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Site Plan Details – Main and Park

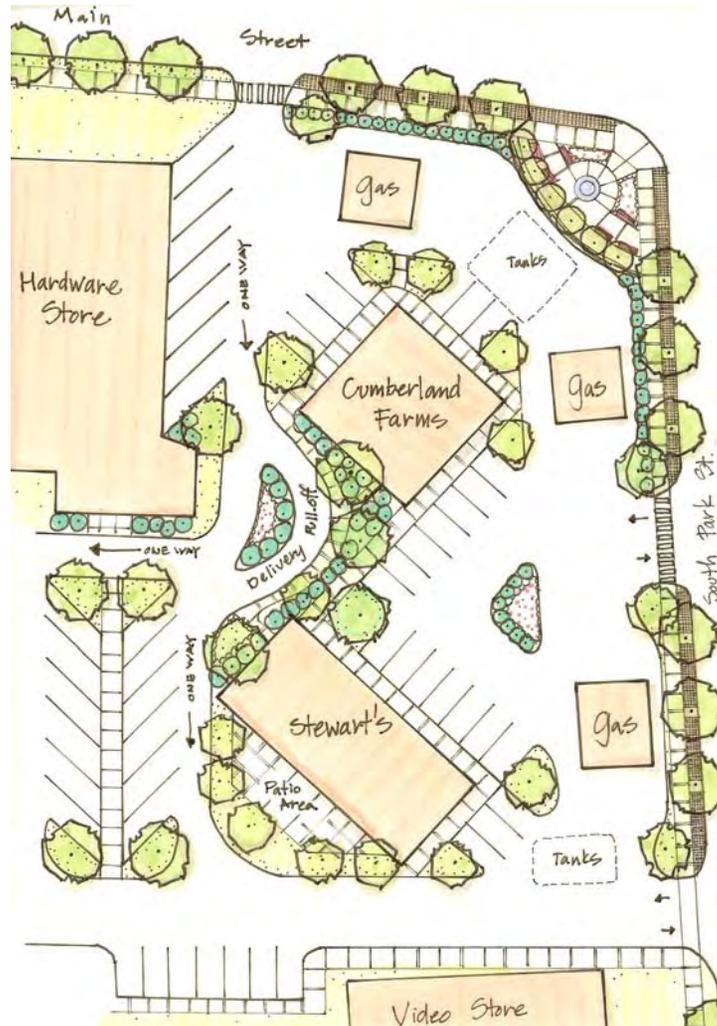


Figure 7: Site Plan for Cumberland Farms corner suggests access management, pedestrian improvements, combined parking and access; and new greenspace.

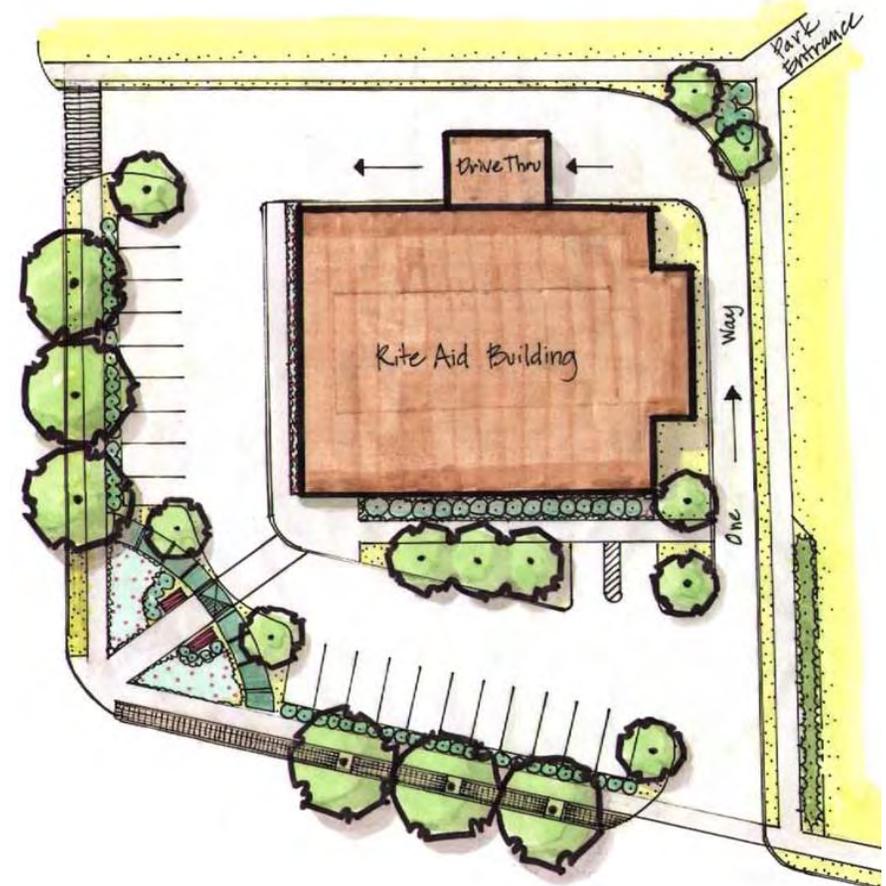


Figure 8: Site Plan for Rite Aid illustrates the focal point featuring a pergola structure modeled after the curving coal trestle that once existed in the railroad yard. The new plan includes a walkway along the north and east property lines that provide easy connections to the Cambridge Valley Athletic fields. As mitigation for providing these improvements, the pharmacy might be allowed to construct a drive through.

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### 2. Main Street Corridor

The Main Street corridor is the commercial spine of the Village, and is also important because it links many of the Village's most significant cultural and historical sites.

Focus areas on Main Street that were discussed in detail during the public workshops are presented elsewhere in this Handbook as separate projects; Main at Park, Main at Union, and the eastern and western gateways. Ideas for the remainder of the Main Street corridor are presented here including the cultural center at the Cambridge Hotel.

Main Street functions well as a central spine in the traditional town plan sense. It has low density residential development at its' eastern and western fringes, and it is denser and predominantly commercial in nature towards the center. The comprehensive plan, by suggesting the establishment of "transition zones" in some of intervening spaces between the three main commercial districts of Union Street, Cambridge Hotel, and Park Street, sets the stage for a unification of the districts in a consistent commercial or mixed use core.

Pedestrian safety is a priority throughout on Main Street. Access management and crossing improvements are needed, as are improved consistent lighting and additional comfort amenities to improve safety and promote walkability. Focal points placed in the major intersections would also help link the districts visually.

Finally, a number of public and private greenspaces exist along Main Street. The spaces are unheralded and used infrequently. They should be organized into an open space system, linked to interpretive trails and alternative pedestrian/bicycle paths and trails. Some of these spaces would provide resting spots and shelter, or become outdoor venues with minimal investments into landscaping and amenities.

#### STRATEGIES:

- Improve Pedestrian Facilities and Continuity Throughout
- Create Greenspace and Public Spaces from Vacant or Underutilized Spaces
- Bury or Relocate Overhead Utility Lines Throughout
- Use Bump-outs and other Interventions to Improve Safety at Union, Memorial Drive, Pearl, Broad, and Washington/Grove.
- Consolidate Access Points and more Clearly Identify Pedestrian Areas



*Figure 9: Main Street residential districts east of Park Street and west of Gilbert Street. A 30' width of pavement is common and should be maintained. If desired a 4' wide bike lane could be striped on both sides of the street. New amenities should include lights next to the curb and street trees at the ROW line.*



*Figure 10: In transition zones and commercial areas where residences exist opposite commercial land uses, two 11' travel lanes and two 7' parallel parking lanes are found. This standard should be consistently applied throughout mixed use areas. New amenities should include lights, benches, planters, etc. and narrow street trees along the curb line.*

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Intersection Improvements:

- **Memorial Drive:** Textured pavement in the intersection and mountable curb bump-outs on both sides of Memorial. Cross walk striping all three connections. Eliminate the bank exit on Main and relocate it onto Memorial as the bus conflict will no longer exist. Create sidewalks and plant street trees on both sides of Memorial.
  
- **Pearl Street and Saint Luke's Place:** Textured pavement in the intersection and mountable curb bump-outs on both sides of Pearl, one side of Saint Luke's Place. Cross walk striping all four connections. The bump-out next to the fire station is shortened for access to the entrance.
  
- **Railroad and Broad:** The road corridor is narrowed from Broad to the hardware store entrance drive to 24' feet wide. Striped crosswalks should be installed at Broad and in front of the hardware store as shown. The entire narrowed area could be textured pavement. Additional street trees and lights should be installed. A large plaza is created at Broad Street.
  
- **Washington and Grove:** Mountable curb bump-outs on both sides of Grove and one side of Washington. Crosswalks on Washington, Main and Grove as shown. Additional plaza space is created at the Washington and Main intersection.



*Figure 11: Intersection of Memorial and Main. Pedestrian crossing improvements include bump-outs on the Memorial side.*



*Figure 14: Intersection of Main Street, Pearl Street and Saint Luke's Place.*



*Figure 13: Intersection of Main and Broad Street and Main and Railroad.*



*Figure 12: Intersection of Main and Washington/Grove.*

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### Streetscape Improvements Sketch:

Business owners and patrons felt a livelier street and pedestrian environment would be good for the community and for business. This sketch illustrates some key elements including contrasting pavement patterns with pavers, an interpretive pavement element and the addition of elements such as benches, banners, awnings, hanging plantings and planter boxes.



Figure 15: Streetscape Improvements near Hubbard Hall and Bean Headz

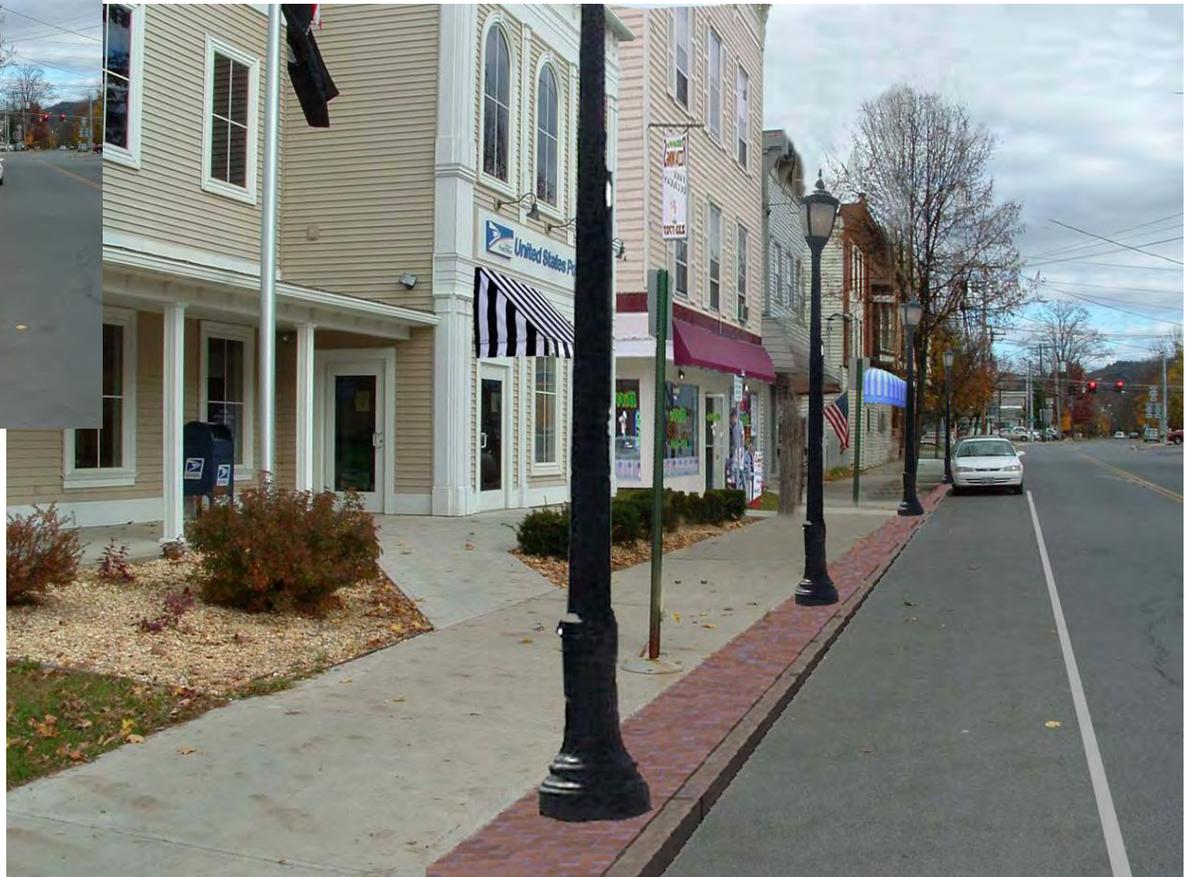
## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### Before and After Simulations of Streetscape Improvements and Infill Development on Main Street



*Photograph 5: Before*



*Figure 16: Photosimulation of Streetscape and Infill Commercial Development*

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### Before and After Simulations of Streetscape Improvements on Main Street Residential Areas



*Photograph 6: Before*



*Figure 17: Photosimulation of Streetscape in Main Street Residential Areas*

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### 3. South Park Street Gateway and School Entrance

Visitors to Cambridge from the Capital District and south get their first impressions of the Village driving north on Route 22, (Park Street). The landscape of the highway on Park, from the Village line to the Cambridge Central School entrances, forms a critical image of Cambridge. This landscape does not, at present, accurately represent the Village

Considerable discussion of this gateway took place during the public workshops focused on how to make a new and better first impression here. Among issues that needed to be addressed is lack of sidewalks, safe crossing for school children, better gateway identification, mitigation of a proposed school bus lot at the Village entrance, creation of public greenspace, and creation of visitor information center.

#### STRATEGIES:

- Provide Additional Tree Plantings
  - Create Safer Crossings at the Cambridge Central School
  - Create a Better Sense of Entrance with a Boulevard and Plantings
  - Coordinate with School District Bus Garage Project
  - Address any Traffic Control Issues – adding Left Turn Lanes
- Provide Better Access Management and Aesthetic Improvements – Clearly Identifying Pedestrian Areas
  - Continue Sidewalks to the Supermarket, provide Crossing at Boulevard Entrance to Mark Village Boundary
  - Create Visitor/Information Center at Old Gilbert Street – Create a Promenade along Old Right-of-way to Gilbert

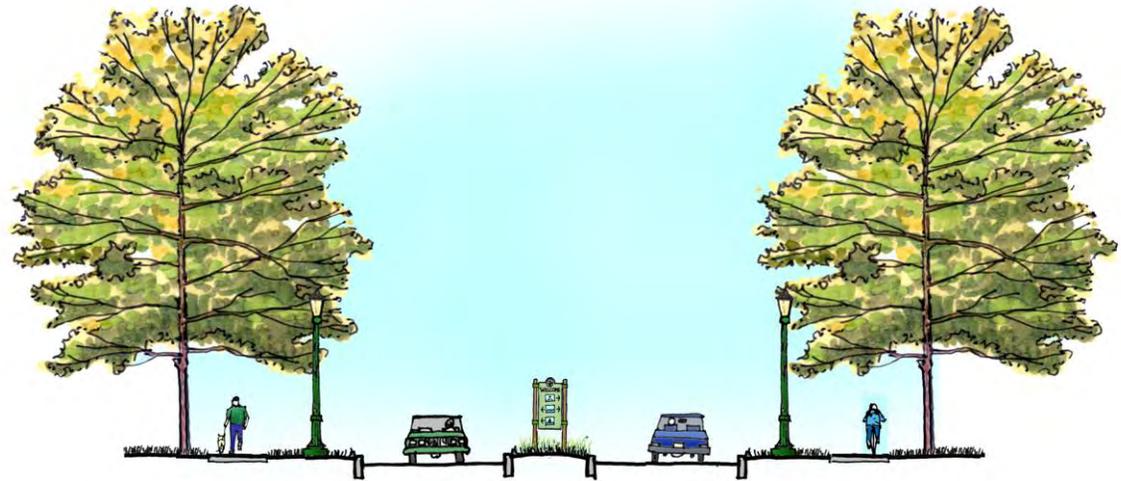


Figure 18: Cross section of the entrance to the Village on South Park. A 10' wide boulevard and two 15' travel lanes should be built. The extra wide road right of way also accommodates an 8' buffer and 6' walk on both sides. New street trees and lights should be installed along the gateway.

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Park Street South Gateway Plan

The plan below illustrates the major impact of streetscape improvements along the Park Street corridor. The splitter islands, sidewalks and street trees utilize extensive public right-of-way space along Park, Gilbert, and Old Gilbert. Other public rights-of-ways are used to create the visitor center site.

Significant pedestrian safety improvements are accomplished for students and other pedestrians with new crosswalk features, sidewalk extensions and a new student drop-off. A left-turn lane is installed at the northern school entrance to enhance safety.



Figure 19: The South Park Street Gateway and Corridor Improvements at Cambridge Central Schools

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### 4. Hubbard Block and the Cambridge Hotel

Hubbard Block, the Freight Yard, and the Cambridge Hotel occupy the portion of the Main Street Corridor that is recognized as the “cultural center” of Cambridge. The area is centered on the railroad crossing of Main Street and is also near the geographic center of the Village. Vacant land exists adjacent to the Main Street corridor to the north and south, much of which is zoned industrial. Some of these lands present an opportunity to create public open and could also serve to create nearby off-street parking.

The importance of this district is evident in the efforts of the Community Partnership, a community based organization that has commissioned the development of conceptual designs for the freight yard. Prepared in 2003 by Jeffrey Goldstone and Jack Byer, the plans increase public greenspace, create common off-street parking areas and provide space for outdoor markets and performance events. When completed, the project will enhance the Village’s “Cultural Center” and will become a catalyst for the establishment of other ventures elsewhere in Cambridge.

The freight yard project will provide a centrally located base for the interpretative system of the Village. A workshop discussion group suggested creating an historic/interpretive trail center here, which could be accommodated in one of the

existing buildings or nearby at the Historical Society.

Gateway signage would direct visitors in the freight yard where they would park and take walking tours of the Village. There will be a need for expanded parking nearby, perhaps south of Main Street, including accommodating large tour buses.

A great deal of significant local and regional history could be interpreted, including many nationally significant cultural changes.

#### STRATEGIES:

- Assist with the Development of the Freight Yard Project
- Expand on Freight Yard Plan Ideas, Cultural Center
- Improve Pedestrian Safety at Broad Street and Main Street Intersection
- Better Organize and Expand Parking On-Street and Off-Street
- Develop Landscape Plans for Green Areas
- Create Historic Interpretive Trails/Visitor Center
- Install More Appropriate Street Trees
- Amenities here will Set the Precedent for Main Street Corridor

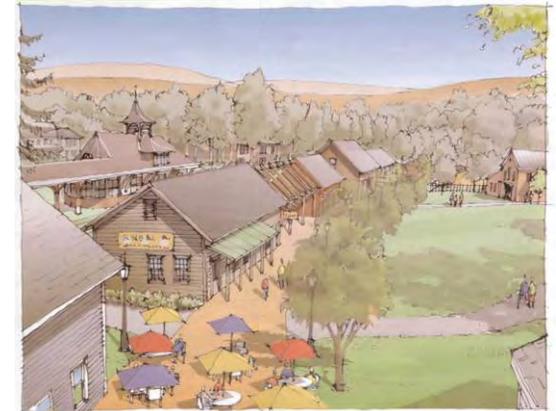


Figure 20: A sketch rendering of the finished Freight Yard Project by Eric Whitting.



Figure 21: Cambridge Freight Yard Plan.

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Site Plan Details – Cambridge Hotel Green:

A privately owned green is maintained by the owner of the Cambridge Hotel in the 'cultural center' of the village. This space has functioned as a public green for over 100 years. The recent revival and renovation of the Cambridge Hotel has re-established an important and significant historic function, as well as preserved a major historical architectural element for the village.

The plan for the Green should relate to the potential for the space to function as an outdoor room connected to receptions and other functions at the hotel, while maintaining a sense of openness. It should also be clearly available for use by the general public when not in use for a hotel function.

The plan illustrates a strong visual connection and a functional pedestrian connection to the green. A walk crosses Broad Street adjacent to main entrance and terminates in a gazebo in the green. The gazebo is surrounded by flower gardens and a sense of enclosure is accomplished with a low hedge. Placement of the gazebo should be carefully considered to allow for visibility along a vista on Main Street.

The remaining greensward is left to form an outdoor room where a variety of activities could take place, including performances, tented receptions and lawn sports. A sense of enclosure is accomplished with hedge rows. A connection to the future Freight Yard project should be formalized with a paved walkway.



Figure 22: Cambridge Hotel Green Sketch Plan

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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Before and After Simulations of the Cambridge  
Hotel Green



*Photograph 7: Before*



*Figure 23: Photosimulation of Cambridge Hotel Green after improvements*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### 5. Main Street and Union Street

Union Street marks the boundary of the Towns of Cambridge and White Creek. This uniquely Cambridge geographical transition is marked by “Town of ...” signs at the Union and Main Street intersection. The Village’s largest contiguous commercial block exists on the south side of Main here, as does the IGA supermarket, a Village institution.

Access management is needed in several locations at this intersection. Near the intersection a pedestrian crossing of Main Street is made nearly impossible by the almost 70’ pavement width of Main Street. Minimal public greenspace exists and few pedestrian amenities are provided.

Businesses here would benefit indirectly from establishment of the freight yard project, and interpretive trail system, which would route visitors to a major interpretive element at the Main/Union intersection. Local businesses here would provide dining, snacks, refreshment and supplies.

#### STRATEGIES:

- Improve Access Management at IGA and Elsewhere
- Provide Angled On-Street Parking on South Side of Main and West Side of South Union

- Eliminate Excessive Street Pavement Width – Create and Expand Greenspace
- Re-align Intersections for Better Function
- Create Safer Crosswalks
- Create Common Multi-Purpose Areas in Backlots
- Create an Historic Interpretive Kiosk or Shelter – Related to Revolutionary War



*Photograph 8: The excessively wide crossing of Main Street at Union Street is reduced from 70' to 30'.*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Main Street and Union Street Plan

Key features of the plan include significant pedestrian improvements, access management, expanded parking, shared-use parking, an interpretive monument, and streetscape improvements throughout. Reorganized parking on the IGA site utilizes a one-way circulation pattern and 45 degree parking to reduce the amount of parking pavement and create greenspace. Union has been slightly realigned to create a more direct crossing for vehicles.



Figure 24: The Main Street at Union Street Commercial Area Plan

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### Main Street and Union Street Sketches



*Figure 25: The intersection improvements would create an interpretive park at the southwest corner, also creating additional space for an outdoor dining area adjacent the restaurant.*



*Figure 26: Sketch of a suggested Revolutionary War Monument*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### 6. Park Street North Gateway

A mix of rural commercial, large lot residential, agriculture, and landscaped cemeteries characterize the entrance to Cambridge on North Park. A project in its infancy here, the new town hall and skate park will establish a precedent for the identity of this gateway. The Village's purchase of a building and acreage for the establishment of a Village Hall and park will provide a large public recreation area oriented towards youth.

Sidewalks and other pedestrian facilities are lacking, and few street trees have been planted along Park Street. Some of the cemetery fencing along Park is in disrepair and is inappropriate for the historical setting.

Park Street is part of the Route 22 snowmobile trail, and the needs of snowmobilers and bicyclists should be accommodated along the corridor through the gateway.

#### STRATEGIES:

- Enhance Sense of Entrance to the Village, Village Scale with Welcome Sign
- Calm Traffic using Boulevard on Park Street also Creating a Pedestrian Crossing
- Close Union Street from Park to Create "Woodlawn Park"

- Create Park with Skateboard Park, Playground and Nature Trails at Village Hall/Police Station
- Consider Locating Parking behind or alongside of the Village Hall, not in the Front Yard
- Replace Cemetery Fencing with more Appropriate Type along Streets
- Extend Sidewalks on Park Street to the Crosswalk at the Boulevard Entry Point
- Create Bike Lanes on Park Street
- Plant Additional Street Trees along Park Street
- Create a Visitor Information Center and Kiosk at the Village Hall



Photograph 9: The present north entrance to the Village on Park Street/Route 22.

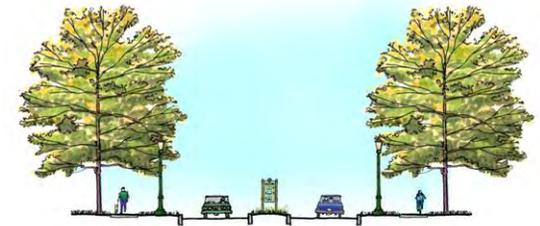


Figure 27: The new gateway would include a 10' wide boulevard, gateway signage, lights, sidewalks and a bicycle/snowmobile trail.

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Park Street North Gateway, Village Hall Park and 'Woodlawn Park' Plan

This plan illustrates three potential projects including the Park Street North Gateway which features a planted splitter island with entry signage.

Union Street between Cemetery and Park Street has been closed to create a linear park which was named 'Woodlawn' at the visioning workshops. This linear park is shown as a promenade with a central focal point and gate features at both ends. The street amenities should be selected from the types used elsewhere in the village.

An active community park is illustrated on the lands recently purchased by the village. Key features include a skate park, basketball court, adventure playground, picnic shelter and nature trails. A clock tower is suggested for the building to help establish it as a public building.

The east side of park includes separate corridors for pedestrians and bicycles/snowmobiles.



Figure 28: The Park Street North Gateway Sketch Plan

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### 7. Main Street at Gilbert Street

Attracting travelers from NYS Route 313 (a.k.a. Maple, a.k.a. Gilbert) and guiding them to Village businesses is a priority at the Main and Gilbert intersection, and it is a key to promoting tourism in the Village. Gilbert Street (NYS Route 313) is a main route from the Capital District and points south to ski areas and resorts in Vermont. Planting a seed with travelers as they head to their destination might encourage a stop in Cambridge on the return trip or better yet a later trip with Cambridge as the destination. Though Gilbert is not a pedestrian street, amenities for pedestrians should be considered, and should mirror existing styles of distant setbacks from the roads and slate pavement.

Access management at the northwest corner of this intersection is needed, which will result in streetscape and pedestrian improvements.

The speed limit on Gilbert Street is 40 MPH, and traffic flows noticeably faster than the posted speed limit, as the road is designed for higher speed. Visual cues to pedestrian crossings will be critical at this intersection to calm traffic and maintain pedestrian safety. Traffic calming such as textured pavement and lane narrowing are strongly recommended measures.

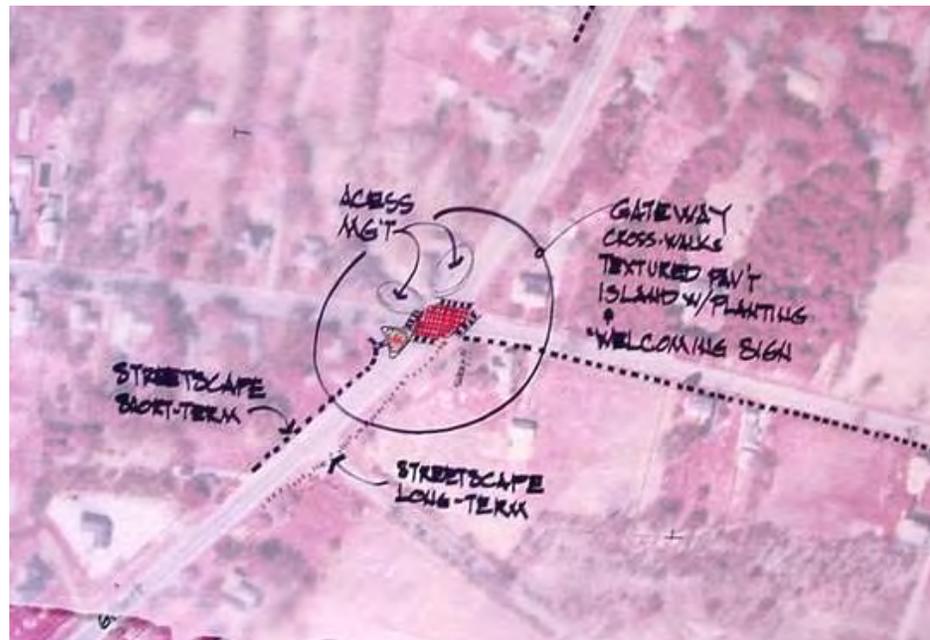
#### STRATEGIES:

- Calm Traffic which Exceeds the 40 MPH Limit

- Enhance Rural Feel of Entry Point
- Create an Entry Feature at the Pork Chop
- Direct Visitors to Turn Left on Main Street
- Continue Sidewalks East along Main Street
- Improve Pedestrian Safety - Crosswalks
- Return Trip “Capture Signage” for Northbound Travelers



*Photograph 10: The intersection of Main Street with Gilbert Street is unheralded at present.*



*Figure 29: Illustration of traffic calming and features intended to capture tourists traveling on Route 313.*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### 8. Main Street West Gateway

Entering Cambridge on Main Street from the west is a unique experience reminiscent of the classic New England village. The scenic hamlet of Coila, which precedes the village entrance and the surrounding agricultural landscape, is mostly unspoiled by modern sprawl. A gently curving route passes over a stone-walled bridge, past a small pond and past a pork chop marking a fork in the road. At this point, the western residential district of the Village comes into view creating a wonderful natural gateway. A few feet further ahead on Main, to the left, is one of the last remaining yellow brick roads in Cambridge.

This gateway has considerable curb appeal; however the pond site is overgrown and lacking amenities. Sidewalks, street lights, crosswalks and other typical heralds of a village are lacking as well.

The Village should consider purchase of the pond site and some adjacent properties to create a maintained public open space.

#### STRATEGIES:

- Enhance New England Village Feel of Entry Point
- Create an Entry Feature at the Pork Chop – West Cambridge Road

- Create a Public Park at the Tannery Pond
- Clean and Repair the Yellow Brick Road
- Create a Major Interpretive Element – Covered Bridge at the Pond
- Utilize Agricultural Plants wherever Possible
- Establish Parking for Trails and Park at the Pond



*Photograph 11: The western gateway to Cambridge on Main Street.*



*Photograph 12: Flax, a historic textile product of Cambridge could be planted in the pork chop marking the Village's entrance.*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### Main Street West Gateway – Sketch Plan

The western Village entrance has the potential to become a major public recreation open space as well as the most attractive gateway. The Coila Road is one of the last remaining yellow brick roads in the Village next to a large vacant parcel which includes Tannery Pond. Other existing amenities include a stone bridge over Cambridge Creek and a pork chop. During the workshops a covered bridge was suggested as a key element, though the original location was not feasible. The new location situates the bridge over a dam, making a functional trail connection from a future 'Cambridge Creek Greenway Trail' that would cross Main Street and Center Cambridge Road continuing west on Main Street, and connecting to existing trails on the McClellan Hospital lands.

The pork chop becomes a welcoming feature that celebrates the Village's agricultural history with an antique farming implement and a planting of flax.

Purchase of an additional parcel could provide a site for a small parking area.



Figure 30: Main Street West Gateway - Sketch Plan

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### Before and After Simulations of the Main Street West Gateway



*Photograph 13: Before*



*Figure 31: Photo simulation showing minor improvement of the site.*

## **9. Maple Avenue Gateway (near Spring Street)**

Participants in the Visioning Workshops did not consider this gateway to warrant the major gateway treatments suggested for the other Village gateways. The use of screening along the western side of the right-of-way and extension of sidewalk through Spring Street were suggested. The main recommendation here is to provide amenities and signage that create a prelude to the Main Street intersection.

### **STRATEGIES:**

- Enhance Sense of Arrival
- Plant a Dense Tall Hedge Row along Western Edge of Road
- Extend Sidewalk along West Side to Spring Street
- Employ a Majority of Gateway Features at the Main Street Intersection



*Photograph 14: The existing Route 313 gateway to the Village.*

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### 10. Varak Park

An attractive ornate pedestrian bridge over White Creek and a slate walkway at Varak Park suggest the existence of some more substantial victorian themed public garden in the past. This significant private greenspace once had a fountain and flower gardens. Establishment of gardens and recreation of the fountain and other historic amenities would create a major enhancement to Main Street. Neighboring residences have attractive architecture, but would also benefit from some landscape improvements, replacement of overgrown ornamental plants, and street-side amenities. Most importantly, the owners of Varak Park might be coaxed into creating a gazebo or small amphitheater for intimate “garden” performance events.



*Photograph 15: The covered pedestrian bridge at Varak Park crosses the Owl Kill. Main Street passes in the background*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

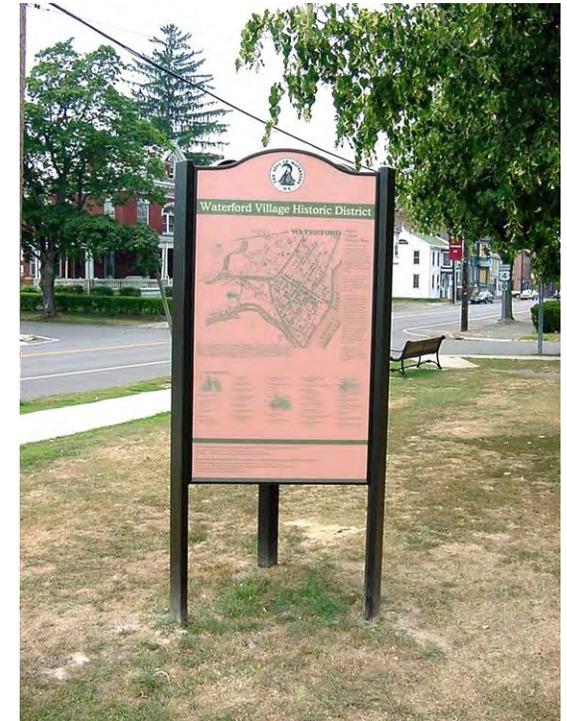
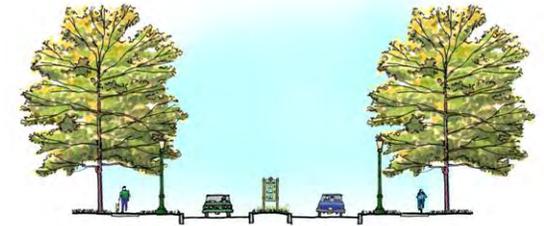
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### D. Implementation Measures

Measures presented in this section are intended to provide additional detail for implementing streetscape improvements through the corridors. Specific recommendations for how and where they are to be used are made for each.

Traffic calming measures have been adapted from standard accepted practices as described in [Traffic Calming: State of the Practice](#), copyright 1999 by the Institute of Traffic Engineers (ITE) and the Federal Highway Administration (FHWA). The measures presented are adapted to the unique conditions of the street corridors of the Village of Cambridge.

1. **Standard Details** – includes details for intersections, walks, parking, etc. ...
2. **Street Furnishings** – presents recommended styles, materials, color and accessories for street furnishings to be used throughout
3. **Additional Planning** – provides information about future planning efforts to be considered to advance the objectives of revitalizing the street corridors



# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### 1. Standard Details

The standard details presented in this section are intended to guide the development of the main road corridors. In general, these details are based on accepted practices that have been adapted for the unique existing conditions of the Village of Cambridge. The intent is to make minor adjustments to the street configurations, while keeping what is familiar.

The details are based on the input received from the public during the workshops. Styles, materials and locations for street amenities such as benches, light poles, and pavements, are all based on workshop discussions and meetings with the technical committee.

These standards are presented as a guide and should be applied with some flexibility and leave opportunities for re-interpretation by others.

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### a. Cross Sections

The following cross sections can be applied to a wide range of settings to help improve pedestrian safety and create a more consistent street appearance. These cross sections apply to locations not covered by specific recommendations contained in the previous section (C).

- **Main Street Residential Areas:** The figure to the right illustrates the recommended cross section which replicates the existing should remain the standard on Main Street west of Myrtle and east of Park. The cross section consists of two 15' travel lanes and five foot wide walks offset three feet from the curb. Light poles should be located between the curb and walk, trees outside the walks.
- **Main Street Transition Zone:** The figure to the right illustrates the recommended cross section in the transition zones between Park Street and Union Street. It replicates the cross section common to Main Street with two 11' travel lanes and two 7' wide parallel parking lanes. Light poles should be located between the curb and walk, trees outside the walks.



Figure 32: Main Street Residential



Figure 33: Main Street Transition Zone

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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- **Park Street Residential Zones:** The figure to the right illustrates the recommended cross section for Park Street Residential areas. The cross section consists of two 15' travel lanes, two 4' bicycle lanes and six foot wide walks offset five feet from the curb accomplished due the wide road right-of-way. Light poles should be located outside the walk, trees between the walks and curbs.



Figure 35: Park Street Residential Zone

- **Park Street Transition Zones** The figure to the right illustrates the recommended cross section for the transition zones along Park Street. The cross section consists of two 12' travel lanes, one 8' wide parallel parking lane, one 8' wide bike lane and five foot wide walks offset three feet from the curb. Light poles should be located between the curb and walk, trees outside the walks.



Figure 34: Park Street Transition Zone

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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- **Gilbert/Maple Rural Residential:** The figure to the right illustrates the recommended cross section for Gilbert Street. The cross section consists of two 15' travel lanes, two 8' wide bicycle/snowmobile lanes and five foot wide walks offset eight feet from the curb. Light poles and trees should be located between the curb and walk.



*Figure 36: Gilbert/Maple Rural Residential*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### b. Intersection Details

The following intersection details can be used on several intersections not covered by specific recommendations in the previous sections.

- **Major Street – Major Street Intersection:**

Where two major streets intersect, there is typically a wide expanse of pavement for pedestrians to cross. The figure to the left illustrates a narrowing of both roads to a recommended minimum width of 22'. The turning radius provided is 15' but can be expanded to 25'. Mountable curbs are recommended for all bump-outs. The pedestrian crossing is marked by white striping on the pavement and is 10' wide. A textured pavement center island is recommended.

- **Major Street – Minor Street Intersection:**

Where a major street intersects with a minor, there is typically a wide expanse of pavement for pedestrians to cross at the major street. The figure to the left illustrates a narrowing of the major street to a recommended minimum width of 22'. The turning radius provided is 15' but can be expanded to 25'. Mountable curbs are recommended for all bump-outs. The pedestrian crossing is marked by white striping on the pavement and is 10' wide and a textured pavement center island is recommended.

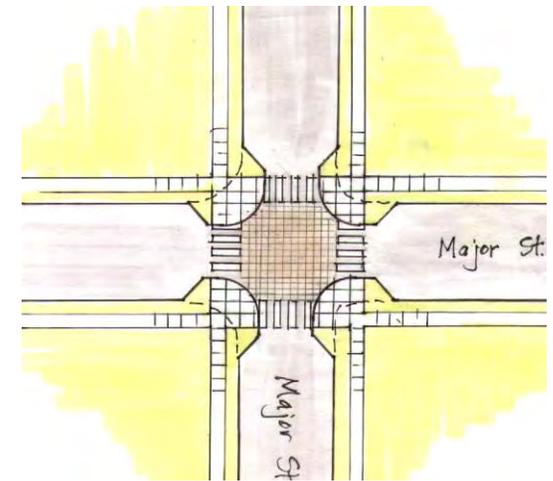


Figure 37: Major Street at Major Street Intersection

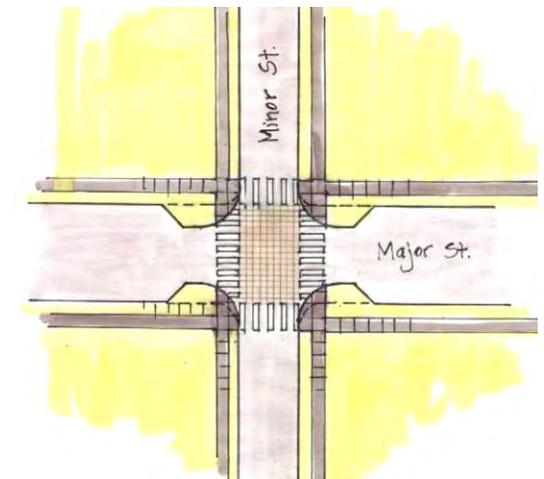
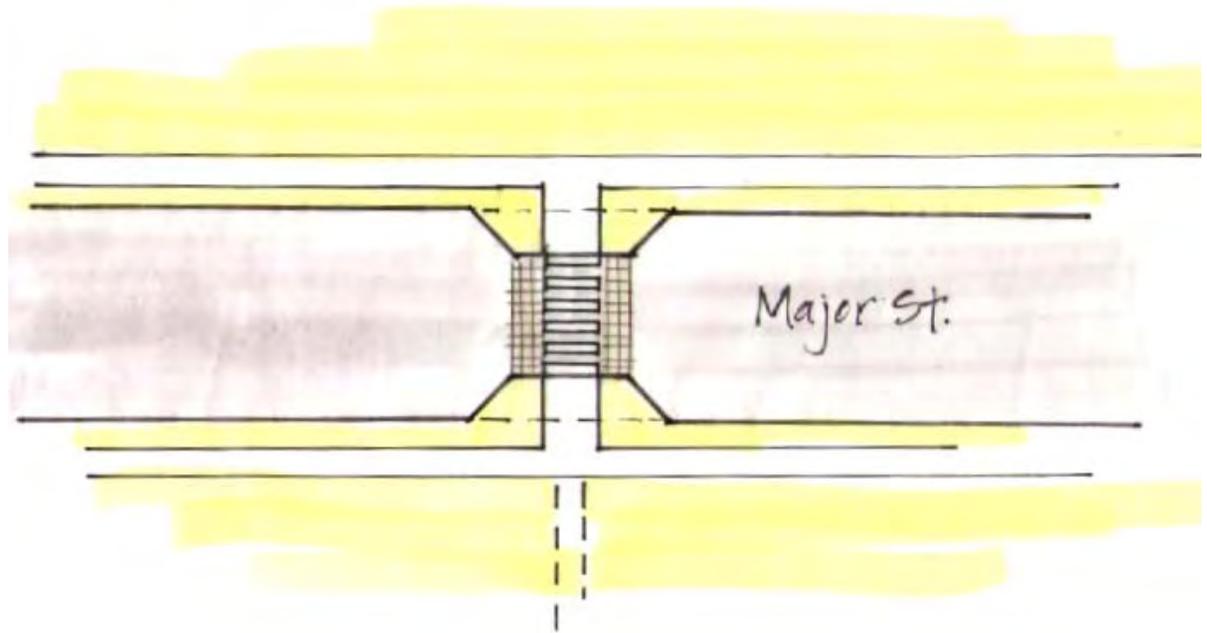


Figure 38: Major Street at Minor Street Intersection

### **c. Mid-block Crossing**

This detail could be used to create a mid-block crossing of Major Street. The figure to the right illustrates a situation similar to Main Street or Park Street with two travel lanes and two parallel parking lanes. This detail is recommended for an instance where there may be a pedestrian desire to cross the street in mid-block, at Varak Park for example.

Similar to the previous intersections in design, the pavement width is narrowed to a recommended minimum of 22'. The crossing is marked by a 10' wide white striped crosswalk with a band of texture pavement on both sides.



*Figure 39: Mid-Block Pedestrian Crossing*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### d. On-Street Parking

On-street parking exists throughout the Village of Cambridge. Parallel parking is the predominant pattern, with the alternative practices of angled or perpendicular parking at the Main and Union Street intersection. Formalizing the on-street parking by way or pavement striping could increase the available on-street parking capacity. In areas where excessive pavement exists, formalization of parking will reduce the pavement to a minimum required for parking, yielding additional green space or pedestrian space.

The following dimensions should be used for on street parking arrangements.

- **Parallel:** Recommended striping for on-street parallel parking is a 18-23' long by 6-8' wide parking stall. For Cambridge the recommendation is to provide a minimum stall of 22' long by 7' wide.
- **Perpendicular (90°):** This on-street arrangement is only recommended for low traffic areas where perpendicular parking is an existing pattern.

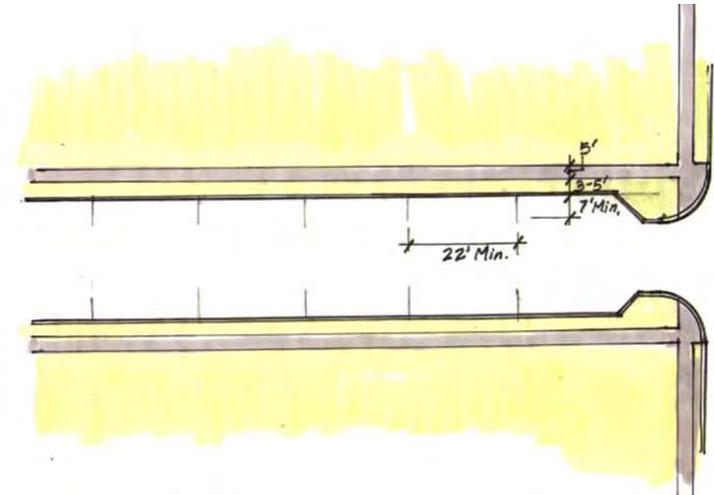


Figure 40: On-Street Parallel Parking

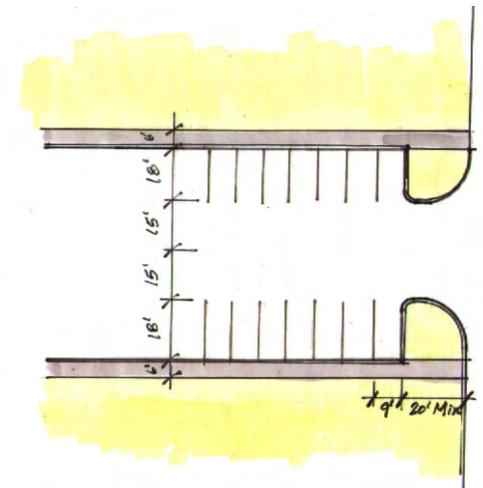


Figure 41: On-Street Perpendicular Parking

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

- 60° Angled Parking – Pull In:** This configuration is shown in detail in the sketch plans for the Main Street at Union Street focus area. The dimensions shown are recommended minimum dimensions.
- 60° Angled Parking – Back In:** An innovation that should be considered by the Village is the back-in 60 degree angled parking. This parking has a ‘learning curve’ stigma, but has been demonstrated as significantly safer in urbanized areas.
- 45° Angled Parking – Pull In:** A variation of 60 degree parking that can be very effectively used on narrower, one-way streets.

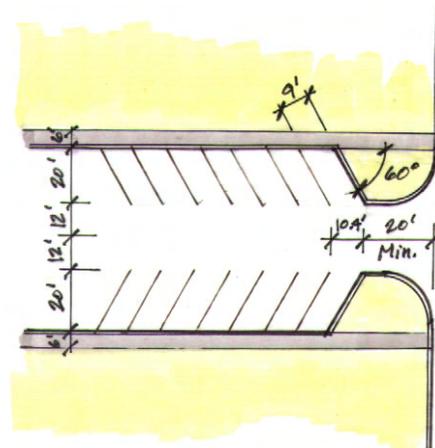


Figure 43: 60° Angled Parking – Pull In

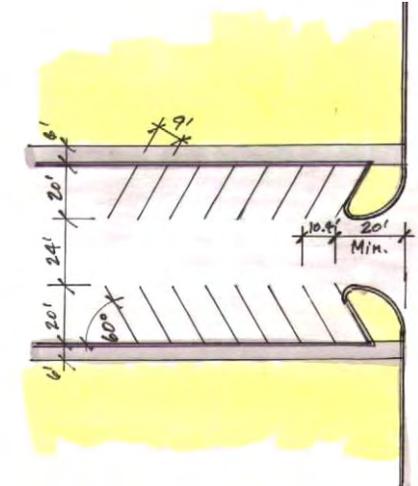


Figure 42: 60° Angled Parking – Back In

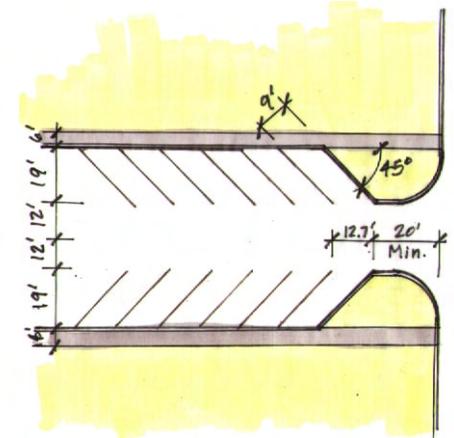


Figure 44: 45° Angled Parking – Pull In

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

### e. Off Street Parking

The following dimensions are recommended for creation and reorganization of off-street parking lots. Formalizing the off-street parking could increase the available off-street parking capacity. In areas where excessive parking pavement exists, formalization of off street parking will reduce the pavement to a minimum required, yielding additional green space or pedestrian space.

- **Standard - two way circulation:**  
Recommended for most off street parking and common parking areas, this arrangement is the most efficient available configuration for lots.
- **60° Angled – one way circulation:**  
Recommended where some spatial limitations exist and where one-way circulation will be used.
- **45° Angled – one way circulation:**  
Recommended for extremely limited spaces and where one-way circulation will be used.

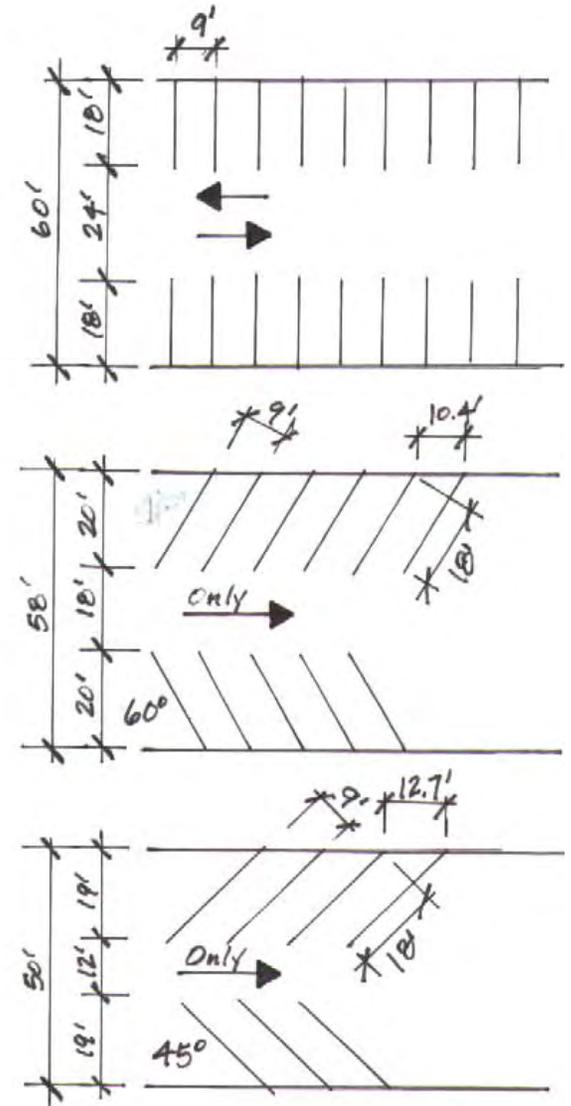


Figure 45: Off Street Parking Dimensions

## **2. Typical Street Furnishings**

The typical furnishings presented are based on the input received from the public during the workshops. Styles, materials and locations for street amenities such as benches, light poles, and pavements, are all based on workshop discussions and meetings with the technical committee.

These standards are presented as a guide and should be applied with some flexibility, leaving opportunities for re-interpretation by others.

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### a. Sidewalks

#### Recommendations:

##### 1. Details

- a. Width – 5’ Min., 6’ Typical

##### 2. Materials

- a. Concrete – light broom finished
- b. Pavers – yellow brick over concrete
- c. Pavers – concrete interlocking
- d. Slate – natural or faux concrete

##### 3. Articulation

- a. Contrasting bands
- b. Scoring patterns – 5-6’ on center
- c. Paver patterns – herringbone in roadway for strength
- d. Bands of pavers at concrete joints

##### 4. Interpretive Elements

- a. Embossed symbol
- b. Names, events, dates
- c. Color coding
- d. Patterns/Designs

##### 5. Accessible Measures

- a. Per AADG Standards



*Figure 46: Pavers articulated with contrasting bands*



*Figure 47: Historic slate walks, (concrete 'look-alikes' are more skid resistant)*



*Figure 48: Yellow brick roads are found throughout eastern NYS*

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### b. Lights

#### Recommendations:

The poles and luminaires used in front of Hubbard Hall are an established precedent for Main Street. They are historically sensitive, attractive and appropriate and should be used throughout the Village for pedestrian areas.

#### 1. Details

- a. Pedestrian areas – see figure at right
- b. Area lighting – off street parking use same where visible from street.

#### 2. Materials

- a. Cast Iron
- b. Molded Fiberglass – not on roadways

#### 3. Accessories

- a. Banners
- b. Cross Arms
- c. Ash urns
- d. Multiple Luminaires – Commercial Areas
- e. Brackets for hanging planters

#### 4. Location

1. See site plans, cross sections
2. General – between curb and walk where on-street parking exists
3. Residential – between walk and prop. line where no on-street parking exists

#### 5. Spacing

- a. General – 3x height of luminaires
- b. As recommended by manufacturer

#### 6. Pattern recommendations

- a. Cut-off lights with a type 4 distribution
- b. Light levels should be brighter at intersections and pedestrian crossing areas.



Figure 49: Example of a double-headed luminaire



Figure 50: The light standard for pedestrian areas in Cambridge



Figure 51: Pole mounted banners add significant interest to street visual environments

# Village of Cambridge Visualization Project and Design Handbook

## Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### c. Planters

#### Recommendations:

##### 1. Materials

- a. Wood barrel – Carol’s Barrels
- b. Cast metal/concrete
- c. Walled-stone or concrete seating

##### 2. Interpretive Elements

- a. Engraved concrete
- b. Engraved wood

##### 3. Size and Locations

- a. Size – generally 2’ wide
- b. Location – between curb & walk
- c. Location – in front of buildings

##### 4. Plantings

- a. Plants should be drought-tolerant
- b. Evergreens
- c. Flowering shrubs
- d. Perennials – ever-blooming or mixed for all season interest
- e. Annuals



Figure 52: Wooden Planter Box



Figure 53: Barrels



Figure 54: Cast Planter

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### d. Fences

#### Recommendations:

Fencing is primarily on private property but should impart a historic village setting wherever feasible. Historic precedents are for use of stone, iron and wood. In areas where extensive fencing is required and the owner desires to use chain link, a high-quality colored coated with additional ornament should be used.

Street-side fencing should replicate historic styles as much as possible.

#### 1. Materials

- a. Metal picket
- b. Ornamental iron
- c. Wooden picket
- d. Coated chain link

#### 2. Screening Fence

- a. Brick or Stone Piers
- b. Metal pickets
- c. Wooden Trellis
- d. Wooden Stockade



*Figure 55: Wooden picket fencing has a distinctive residential feel.*



*Photograph 16: An attractive screening system for parking lots*

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### e. Hedges

Hedge rows are primarily used to delineate private property in the village. They could be used elsewhere for screening and buffering.

Historically, plant materials such as yews, privets and tall hedge have been used. Most hedging plants require extensive yearly maintenance to maintain their appearance. Certain plants that have a natural columnar growth habit should be used to reduce this maintenance.

#### 1. Locations for Hedges

- a. Residential frontage
- b. Screening Off-street Parking Areas
- c. Screening Street Facing Utility Areas
- d. Natural Fencing for Cemeteries

#### 2. Partial List of Hedging Plant Materials

- a. Privet – requires regular trimming
- b. Yew – some trimming required
- c. Barberry – a lower hedge, spiny
- d. Burning Bush – some trimming required
- e. Forsythia – some trimming required
- f. Holly - spiny
- g. Tall Hedge – low maintenance
- h. Spirea – requires regular trimming
- i. Hemlock – tall, dense, screening
- j. Viburnum – some trimming required
- k. Juniper – tall, dense, pyramidal, spiny
- l. Potentilla – low, some trimming required



*Photograph 17: Hedgerows along Main Street delineate private residential space.*



*Figure 56: Burning bush Hedge in Fall Color*

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### f. Garden Walls

#### Recommendations:

The local tradition of stone and masonry should be repeated whenever possible. Stone walls including slate fieldstone, cobblestone and limestone are found throughout the village.

#### 1. Materials

- b. Dry laid flat stones
- c. Cobbles
- d. Mortared stone
- e. Stone veneers
- f. Precast Concrete segmental retaining walls – tumbled or split face with appropriate colors



*Photograph 18: Dry Laid Stone Wall on Maple Ave.*



*Figure 57: New concrete segmental wall units simulate historic styles; these walls utilized units manufactured in Queensbury by Duke.*



*Photograph 19: Limestone wall along Main Street in Cambridge*

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### g. Benches

#### Recommendations:

Existing public open spaces in the village have a concrete support and wooden slat style of bench. This bench has a long lifespan, cannot be stolen and slats can be replaced. The concrete cast in several different style interpreting historic forms and can be colored or painted.

Participants in the workshops sought a new type of bench for use in pedestrian areas and preferred metal and metal with wooden slats.

#### 1. Materials

- a. Metal
- b. Timber
- c. Concrete with wooden slats

#### 2. Locations

- a. General – between the curb and sidewalk



*Figure 59: Updated version of concrete and wood slat bench*



*Photograph 20: Street side benches found in Cambridge. Original design could use an update.*



*Figure 58: This style of bench by DuMor is favored by the Village as a new standard*

## **h. Trash Receptacles**

### **Recommendations:**

Trash receptacles should match bench and planter styles. Most bench manufacturers also design trash receptacles and other amenities that complement their bench styles. Park areas where concrete benches with timber slats are used should have a wooden slat style receptacle.

#### **1. Materials**

- a. Metal strap
- b. Metal mesh
- c. Conc./timber slates

#### **2. Location**

- a. General – between the curb and sidewalk
- b. Parks, open spaces and commercial areas



*Figure 60: Metal receptacle by Maglin Mfg.*



*Figure 61: Wood slat receptacle by Belson.*

## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### i. Tree Protection

#### Recommendations:

Street tree plantings in urban environments are subjected to numerous stresses including soil compaction and damage from cars, trucks and snowplows. Wounds to trunks can kill portions of the tree and make it more vulnerable to disease and pests.

All existing and new tree plantings within a few feet of where damage can be expected should be protected with tree guards and tree grates. Colors should be keyed to interpretive elements or other street furnishings

A regular maintenance pruning of trees should be considered.



*Photograph 21: Small trees have a fighting chance for survival when protected by tree guards and tree grates. (Photo from Univ. of Florida)*



*Figure 62: Metal slat style tree protection*

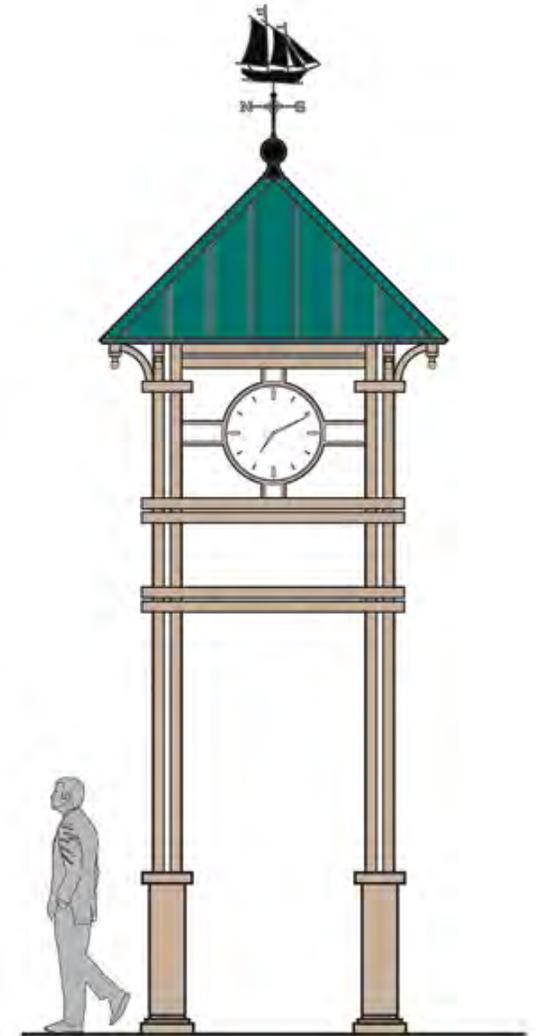
## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### j. Other Furnishings

#### Recommendations:

Street furnishings such as shelters, interpretive signs, clock towers, etc. should be architecturally compatible with the forms in Cambridge. Several manufacturers have product lines that can be adapted or installed as is such as the clock tower to the right.



*Figure 63: A clock tower manufactured by Icon Shelters has the appropriate form and detail for Cambridge.*

### **3. Additional Planning**

The following implementation measures require an additional planning effort beyond the scope of this handbook. It is likely that most of this work can be undertaken by various organizations already in place in Cambridge. All of these efforts can be bolstered with public grant funds to help defray costs.

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## Village of Cambridge Visualization Project and Design Handbook Main Street, Park Street and Gilbert Street/Maple Avenue Corridors

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### **a. Certified Local Government (CLG)**

The NYS Historic Preservation Office administers a number of important historic preservation programs, one of which is the CLG program. The CLG program was established by the federal National Historic Preservation Act to encourage local governments to engage in historic preservation. The key element of a CLG is the development of an Action Plan for historic preservation that presents local strategies to attain historic preservation goals. For New York municipalities there are several benefits for participating including:

- Grant funding for CLG projects
- Membership in the National CLG Network
- Technical assistance from the SHPO
- Direct local involvement in preservation activities
- Training for local municipal officials and other associated with the CLG

### **b. Uniform Signage System**

Development of a uniform signage system to provide a standard format for the multitude of warning signs, information signs, traffic control signs and other signage. In addition, the study would develop systems for interpretive signage, trail signage and historical/cultural markers. The study would also suggest standards for commercial and residential areas.

### **c. (BID) Business Improvement District**

Thousands of business improvement districts have been established across the United States with great success and highly beneficial results. In the City of San Diego where some of the nation's first BID's were established, 18 separate such entities operate.

A BID is a special assessment district established by a community to promote and improve business. Generally initiated by local businesses, each is assessed a yearly fee, based on an agreed-upon scale. All assessed fees are expended benefiting businesses in the BID. The collected funds can be matched by the community (as an incentive) and together those become a significant cash match towards state or federal community development grants requiring a match.

For example, if the BID assessed its members a total of \$10,000 later matched by \$10,000 from the community's general fund, a \$20,000 fund becomes available. That \$20,000 can be used to leverage additional public funding from a matching grant. A 50/50 matching grant creates a \$40,000 pool, while at a 25/75 match an \$80,000 cash fund for the BID is created.

In addition the BID will have access to certain funding programs for which municipal governments are ineligible.

The Cambridge BID should be tailored to the unique needs of business in the Village and funds expended to:

- Purchase, construct, improve and maintain shared-use parking areas
- Make streetscape improvements including purchase of benches, lighting, trash receptacles, planters and signage
- Establish, sponsor and promote special events in the BID
- Market and promote

Establishment of a BID is usually initiated by a petition of local businesses to the municipality for the creation of a local law. Many municipalities provide public hearings and will establish the BID unless 50% or more of the affected businesses protest the assessment.

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### **d. Business Demand Study/Analysis**

One of the first projects undertaken by the Cambridge Business Improvement District might be to commission a Business Demand Study. The study would assess local service needs, demographics and trends; then analyze how existing businesses are responding to those factors. The results of this study would be a series of recommendations based on:

- Underserved retail/service needs of the community
- New business enterprises that would compliment those that exist
- Marketing and promotion needs
- Special events that would benefit local business
- Themes or branding for the BID

### **e. Traffic and Access Study**

Most of the changes to the Village street network resulting from this plan will have little impact on traffic, other than to calm it and make streets safer for pedestrians. Some functional changes could occur as a result of potential changes to flow in off street parking and common parking areas. Other changes could impact residential areas, where new common parking areas are created that will create new connections to residential streets. A village-wide traffic study should be considered to model those impacts and suggest mitigation for them. The study would include:

- Analysis of the impact of off-street common parking areas
- Suggest control measures to improve pedestrian safety
- Suggest measures to reduce traffic impact from common parking lots on residential areas
- Investigate creation of one-way flow for certain streets
- Investigate the potential for new traffic signals

### **f. Historic and Interpretive Plan**

Cambridge has been in existence since 1761, and has been a part of every important era of US History. In spite of loss of some significant historic treasures, the Village remains a potential living history museum that could present interpretation of many of these important eras.

The key is to develop an understandable and coherent system that organizes those interpretation opportunities. The plan would:

- Inventory and map local significant historic, heritage and cultural sites
- Establish themes and organize sites into interpretive trails and driving tours
- Suggest content and locations for major interpretive displays and informational kiosks
- Suggest outreach materials and promotional strategies
- Develop brochures and guides

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### **g. Village Open Space & Recreation Plan**

During the development Comprehensive Plan and development of this Handbook, several public recreation needs have been identified. Conversely, there are a number of vacant or underutilized spaces in the Village that could be used to meet those needs. This plan would follow a similar process to that of the Comprehensive Plan and this Handbook including Stakeholder Workshops and a final Open Space and Recreation Plan. The Plan would include:

- An Exhaustive Inventory of Open Spaces and Open Space Needs throughout the Village.
- Analysis of Recreation needs and how to meet them.
- A series of Recommendations and Implementation Measures.
- An Action Plan.
- Funding Strategies.

### **h. Bicycle and Snowmobile Plan**

While the visualization project includes some recommendations for bicycles and snowmobiles on the main road corridors, the Village would greatly benefit from undertaking a bicycle and snowmobile route plan, perhaps in conjunction with the Recreation and Open Space Plan. This plan would recommend how and where to best:

- Respond to desired routes for bicyclists and snowmobilers utilizing available open spaces and corridors
- Specific recommendations for trailheads, informational signage, directional signage and user amenities

## **E. Village Code Changes**

### **1. Form-Based Code**

To paraphrase a famous quote, “ ... the problem with our communities today is that they are engineered before they are designed ...” This “engineered” look is prevalent in most post-World-War II community development and is the result primarily of Euclidian Zoning Ordinances, adopted by most every community in the country. For years design professionals have been touting alternatives that put community design before engineering standards, however the communication of this design-based form has not been something easily grasped. In it’s latest “form” design oriented code is being called “Form-Based Code,” a distinction that fits the newest versions of Form-based Code have been developed to graphically represent what was previously written standards, which makes the code more understandable and more accessible to layman. Beyond the ease of interpretation, there are a number of other advantages to form based code (adapted from “Form First” by Peter Katz, *Planning*, v. 70, n.10):

- FBC states what you want, rather than what you don’t want
- “FBC’s encourage public participation because the public can [visualize]what will happen”

- “FBC’s result in a diversity of architecture, materials, uses and ownership”
- FBC’s work well in established communities because they ... define and codify ...” [the existing community fabric]
- FBC’s are more easily interpreted by layman

A good starting point for the development of a Form-based Code for Cambridge would be the architectural standards being currently developed, thereby drawing the future from the past.

## **2. Mixed-use Buildings Code**

An offshoot of the Form-Based Code would be additional revisions to Village code that would permit some flexibility in land uses, unavailable under current Euclidian Zoning. Currently, only one land-use is permitted for a new structure, which does not allow the western tradition of first floor commercial and upper floor residential. Several in-fill opportunities exist for new commercial structures in the Village, and permitting this type of mixed use, would be an incentive to developers. In addition, in certain areas of the Village, it might be appropriate to permit additional principal land uses, i.e., residential on the same parcel.

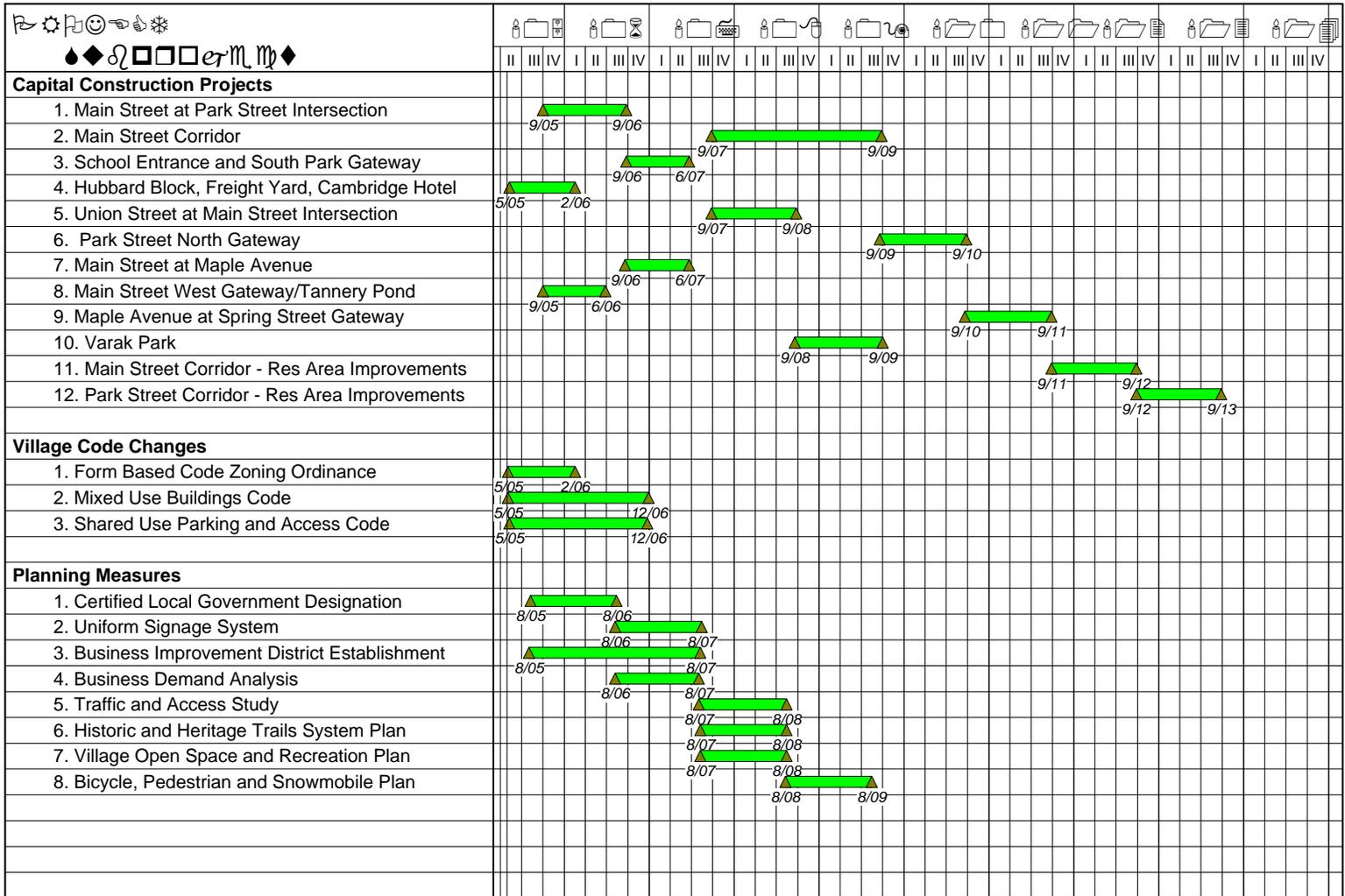
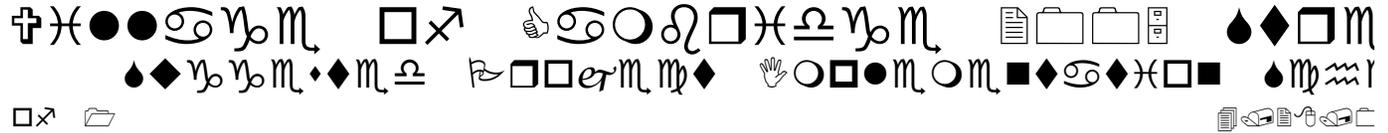
### **3. Shared-Use Parking Code**

Potential incentive to development of shared-use parking would be a “Shared Use Parking” Code. Adapted as part of a Form-Based Code or developed separately, this code would permit developers to utilize parking from off-site to meet their demands. This of course would require cooperation agreements and demonstrations, where appropriate, that parking demand would not overlap in terms of time of day.

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### F. Phasing Plan



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**G. Funding Strategy**

1. Economic and Community Development Programs
  - Governor’s Office of Small Cities Small Cities Grants, Loans, Technical Assistance
  - NYS DOT – Scenic Byways, Transportation Enhancements Program, Transportation Improvements Plan
  - NYS Dept., of Housing and Community Renewal – Home, Housing, Trust Fund
  - NYS Dept., of Housing and Community Renewal – NY Main Street Grant Program
  - Member Item Program
  - USDA Rural Development Services – Utilities, Facilities, Housing, Business Grants and Loans
2. Parks and Recreation Development Programs
  - NYS OPRHP Parks, Recreation and Historic Preservation Grants
  - NYS OPRHP Recreational Trails Program Grants
  - NYS OPRHP Snowmobile Trails Program
3. Transportation Related Funding
  - NYS OPRHP Barns Preservation Program
  - US Land and Water Conservation Fund (Administered by OPRHP)
  - US Soccer Foundation
  - Baseball Tomorrow Fund
  - Bowerman Track Renovation Program
  - Tony Hawke Foundation (Skateboarding)
  - Kodak America’s Greenways Program
  - DOT Transportation Improvements Projects (5 Year Plans)
  - New York State DOT Scenic Byways Program
  - TEA-21 Program
4. Historic Preservation Programs
  - Preservation League of NYS - Preserve NY Grants
  - NYS OPRHP – Certified Local Government Program,
  - NYS OPRHP – EPF Capital Grants
5. Administrative Funding Programs
  - New York State Landmarks Conservancy - Sacred Sites Program
  - National Trust for Historic Preservation
  - National Park Service – Save Americas Treasures
  - National Endowment for the Arts
  - Bender Family Foundation (Administered by the Capital District Community Foundation)
  - Conservation Assessment Program
  - Surdna Foundation
  - JM Kaplan Fund
  - NYS Council on the Arts
  - NYS Local Government Records Management Improvement Fund
  - NYS Environmental Facilities Corp. Financial Assistance to Business (FAB)
  - NYSDEC - Brownfields Opportunities Areas Program (BOA)
  - EPF Brownfields Remediation Program

